



DESIGN AND ACCESS STATEMENT
PROPOSED LIDL FOOD STORE
BULWARK ROAD, CHEPSTOW
JULY 2021



PROPOSED DEVELOPMENT - BULWARK ROAD, CHEPSTOW

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The purpose of this Design and Access Statement is to explain the approach of the design that has been adopted in the application submitted.

This Design and Access Statement has been prepared in line with the requirements of the 'Town and Country Planning (Development Management Procedure) (Wales) (Amendment) Order 2016, which are as follows:

1. "Explain the design principles and concepts that have been applied to the development;
2. Demonstrate the steps taken to appraise the context of the development and how the design of the development takes that context into account;
3. Explain the policy or approach adopted as to access, and how policies relating to access in the development plan have been taken into account; and
4. Explain how any specific issues which might affect access to the development have been addressed".

The Design and Access Statement forms an integral part of the application submission and should be read in conjunction with the Architectural Drawing Package. The DAS is informed by other documents submitted with the application including: Planning and Retail Statement; Transport Assessment; Ecological Assessment; Drainage Strategy; and Landscape Strategy. The conclusions of these reports have been taken into account in preparing the Design

and Access Statement.

The Design and Access Statement provides important and detailed information about the scheme and has been written for a wide target audience, including the Local Planning Authority, Local Residents, Statutory and Non-Statutory Consultee's and Elected Members.

The document takes the reader through the various design stages of the scheme and specifically provides information relating to the following elements:

Assessment

A detailed analysis of the application site and its surroundings, in both physical and social/ economic terms. It is also important to consider the planning policy context relevant to the redevelopment of the site.

Evaluation

An evaluation of the information is collected before the "Assessment" and "Involvement" stages takes place. Based on this information, the Design and Access Statement provides an identification of the constraints and opportunities relating to the redevelopment of the site and identification of the key development principles.

Design

Once the development options have been evaluated and any potential conflicts resolved, the design solution is developed.



PRINCIPAL CLIENT



LIDL GREAT BRITAIN LTD. -
WATERTON INDUSTRIAL ESTATE,
BRIDGEND, CF31 3PH

PRINCIPAL DESIGNER



One Design Architects Ltd
Unit 5 Mercury Quays,
Ashley Lane, Shipley
West Yorkshire
BD17 7DB

This Design & Access Statement has been prepared on behalf of Lidl Great Britain Ltd to support a planning application seeking full planning permission for the demolition of existing, vacant industrial buildings at the former Severn Bridge Social Club; redevelopment of the site to provide a new Lidl food store (Use Class E) with associated car park, landscaping and a new access to the site.

The Lidl development is proposed to serve the Chepstow area, whilst creating extra jobs, alongside redeveloping and enhancing a former social club site.

This DAS sets out the design principles and concepts that have been applied to the development, demonstrating how the context of the development has been appraised and how the design of the development takes the context into account. The DAS further explains how issues relating to access have been dealt with in the context of local policy.

Given Lidl's commitment to invest in Chepstow, the proposed development will, when consented, be constructed at the earliest opportunity. The existing Lidl store on Bulwark Road adjacent to Bulwark post office, will be relocated here.





Lidl, founded in Germany in the 1930s, commenced trading in the UK in 1994 and now operates over 800 stores nationwide. Lidl's ambitious and successful expansion strategy is credited to its retail philosophy centred on principles of simplicity and maximum efficiency at every stage of the business, from supplier to consumer. This focus enables the company to sell high quality products from a limited range of exclusive own-brand and well-known labels at the lowest prices.

Lidl stores sell around 2,500 carefully selected product lines and are able to offer these high quality products at low prices due to the combination of a pan-European bulk purchasing policy and directly owning a number of their key supply chains. This affords Lidl the advantage of immense buying power, enabling the company to achieve significant economies of scale that translate into low prices for the customer.

Evidence of Lidl's success is not only proven through its expanding customer base and customer loyalty but is also compounded by the numerous accolades achieved in recognition of both Lidl's high quality and affordability. Amongst others, Lidl have been voted IWSC Retail Innovator of the Year 2019, Retail Week Awards Best Value Retailer 2019 & Loved by Parents Supermarket of the Year 2019.

This year Lidl have already succeeded in being awarded a multitude of awards.

These include:

Good Housekeeping Reader Recommended 2020 - In-Store Bakery and other products.

World Whiskey Awards 'Own Brand Supermarket of the Year'.

World Vodka Awards 'The Country Winner' for the Rachmanioff vodka.

World Gin Awards 'Own Brand Gin Supermarket of the Year' & 'Supermarket of the Year'.





SITE SIZE AND TOPOGRAPHY

The application site extends to approximately 1.45 Acres/0.58 Hectares.

The principle frontage is to Bulwark Road.

It is proposed that the levels of the food-store & associated car park will be adjusted to improve pedestrian & vehicular access & circulation.

CURRENT USE OF SITE

At present the application site currently contains a social club building along with car parking spaces. The current access into the site will be adjusted to meet highways standards.





Application Site

(Image taken from LDP)



Application Site

(Image taken from LDP)

Planning Policy Appraisal

This section of the report outlines the planning policy framework relevant to the planning application. It sets out the policy context at both national and local levels' focusing in particular on those policies relating to design and access.

Future Wales: The National Plan 2040

Future Wales sets out a spatial strategy as a guiding framework for where large-scale change and nationally important development will be focused over the next 20 years.

Policy 1 'Where Wales will grow' supports sustainable growth in all parts of Wales, with a series of national and regional growth areas, of which Chepstow falls outside of, but appropriate development in towns and rural areas to support local aspirations and needs are encouraged. Policy 6 - 'Town Centres First' indicates significant new commercial, retail, education, health, leisure and public service facilities must be located within towns and city centres. They should have good access by public transport to and from the whole town or city and, where appropriate, the wider region.

MONMOUTHSHIRE ADOPTED LDP (2011-2021)

Policy RET3 - 'Neighbourhood Centres' Indicates development proposals for retail development (use class A1) in designated neighbourhood centres will be permitted provided that the development, either individual or cumulatively with other recent or proposed developments, does not undermine the vitality, attractiveness or viability of town or local centres.

Policy S1 - 'The Spatial Distribution of New Housing Provision' indicates the main focus for new housing development is within or adjoining the Main Towns of: Abergavenny; Chepstow and Monmouth.

Policy S6 - 'Retail Hierarchy' defines the hierarchy of retail centres in Monmouthshire and indicates all new or enhanced retail and commercial development will be focussed in the County's main towns and local/neighbourhood centres, and should be consistent in scale and nature with the size and character of the centre and its role within the hierarchy. Proposals which would undermine the retail hierarchy will not be permitted. Policy S6 identifies Bulwark (Chepstow) as a neighbourhood centre.



Policy RET4 - 'New Retail Proposals' seeks to focus future retail and commercial leisure/ entertainment development in the county designated Central Shopping Area. This is considered essential in strengthening the centres' role/ function in the retail hierarchy, reinforcing their attractiveness to local communities and visitors in maintaining and enhancing their vitality and viability. The policy also supports sustainability objectives by focusing such facilities on accessible locations and contributing to a reduction in travel demand.

Policy S5- 'Community and Recreational Facilities' Indicates development proposals that provide and/or enhance community and recreation facilities will be permitted within or adjoining town and village development boundaries subject to detailed planning considerations. Development proposals that result in the unjustified loss of community and recreational facilities will not be permitted.

Policy CRF1 - 'Retention of Existing Community Facilities' States "The change of use or conversion to other uses neighbourhood or village shops, halls, public houses and other community facilities will only be permitted where:

- a) The local community would continue to be adequately served by facilities to which there is easy and convenient access by means other than the private car;
- b) There is evidence that the facility is not, and could not reasonably be expected to

become, financially viable or the facility, if non-operational has been vacant for a substantial period of time; and

c) Genuine attempts at marketing the facility, whether in use of vacant, have been unsuccessful

Policy S7- 'Infrastructure Provision' states that the infrastructure needed to service and deliver sustainable development must be in place or provided in phase with proposed development. Where existing infrastructure is inadequate to serve the development, new or improved infrastructure and facilities to remedy deficiencies must be provided. Where provision on-site is not appropriate, off-site provision, or a financial contribution towards it, will be sought.

Policy S16 – Transport requires that development proposals should promote sustainable safe forms of travel which reduce the need to travel and increase provision for walking, cycling and public transport.

Policy S17 – Place Making and Design, requires developments to contribute to high quality, attractive and sustainable places.

Policy SD4 – 'Sustainable Drainage' indicates development proposals will be expected to incorporate water management measures, including SuDS to reduce surface water run-off and minimise its contribution to flood risk elsewhere.

Policy NE1 – 'Nature Conservation and

Development' indicates development proposals that would have a significant adverse effect on a locally designated site of biodiversity and / or geological importance will only be permitted where the need for the development clearly outweighs the nature conservation or geological importance of the site and cannot be accommodated elsewhere.

Policy EP1 – 'Amenity and Environmental Protection' indicates development should have regard to the privacy, amenity and health of occupiers of neighbouring properties. Development proposes that would cause or result in an unacceptable risk / harm to local amenity, health, the character / quality of the countryside or interests of nature conservation, landscape or built heritage importance due to the air/light/noise/water pollution, contamination or land instability will not be permitted, unless it can be demonstrated that measures can be taken to overcome any significant risk.

Policy MV1 – 'Proposed Developments and Highway Considerations' indicates all development that is likely to have a significant impact on trip generation and travel demand must be accompanied by a Transport Assessment. Development that is likely to create significant and unacceptable additional traffic growth in relation to the capacity of the existing road network and / or fails to provide a safe and easy access for road users will not be permitted, unless appropriate proposals for related improvements to the highway system or

a contribution towards mitigating traffic management / reduction measures are made.

Policy MV2 – 'Sustainable Transport Access' states that development should make provision for sustainable transport links, including public transport, walking and cycling. Development should link into the existing pedestrian and cycle network.

Policy DES 1 – 'General Design Considerations' all development should be of a high quality sustainable design and respect the local character and distinctiveness of Monmouthshire's built, historic and natural environment. The policy provides a comprehensive design criteria for new development this includes inter alia:

- Ensuring safe, secure places accessible to all members of society, and encouraging walking and cycling.






ACCESS

The existing vehicle access into the site is from Bulwark Road via a new priority junction arrangement for use by customers, staff and delivery vehicles.

Existing bus stops are located along the Bulwark Road, making this an accessible and sustainable location in terms of public and private transport.

Boundaries of the site are defined and comprise of:

- North boundary – Residential
- East boundary – Retail & Industrial
- Southern boundary – Industrial & Retail
- Western boundary – Retail & Residential

 Proposed Site Entrance





VIEW OF THE CURRENT ACCESS ROAD



VIEW LOOKING TOWARDS THE EXISTING SEVERN BRIDGE SOCIAL CLUB

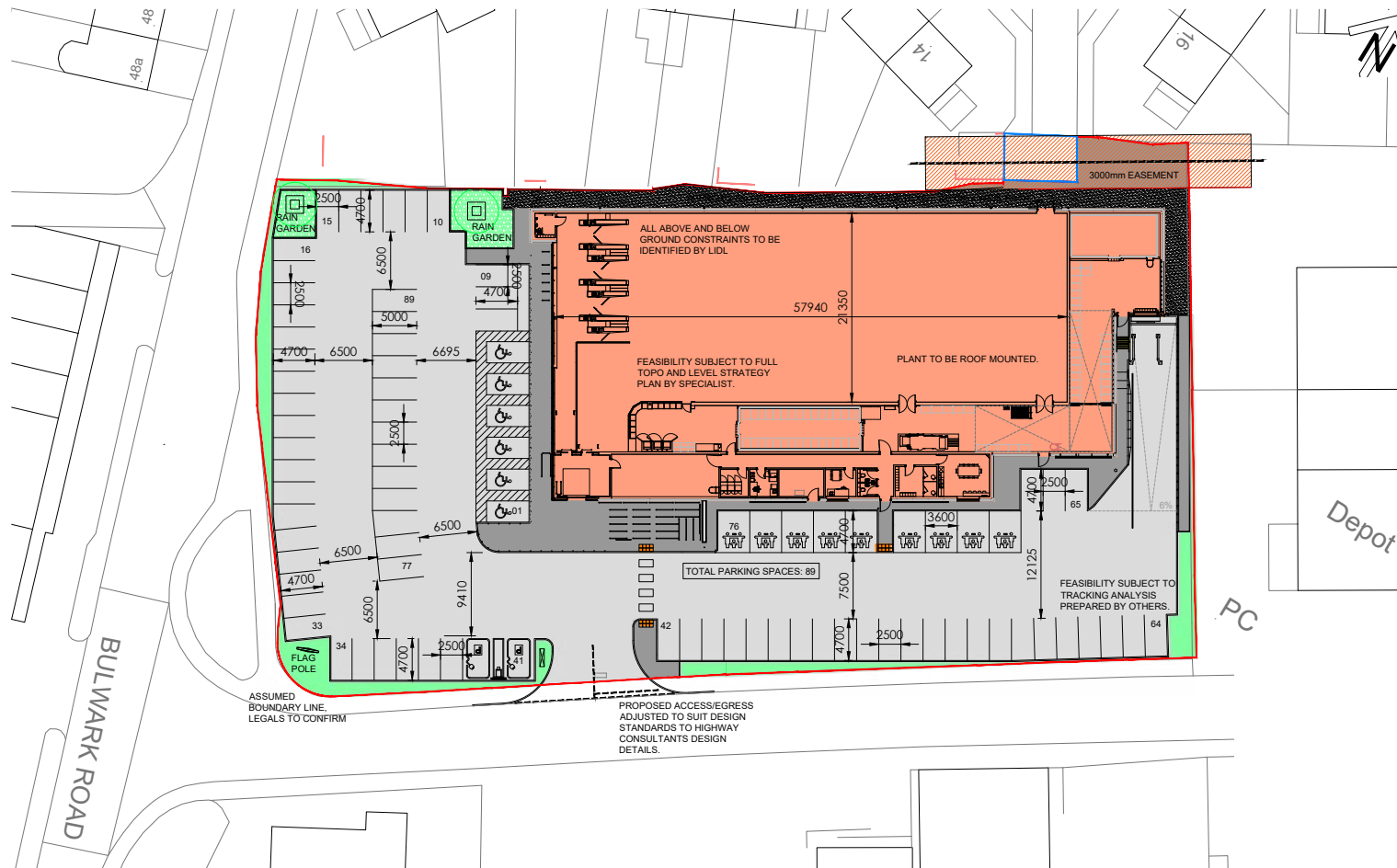
SURROUNDING

The application site is located in an area containing residential, industrial and retail buildings.

Adjacent land uses include, to the:

- North boundary – Residential
- East boundary – Retail & Industrial
- Southern boundary – Industrial & Retail
- Western boundary – Retail & Residential





OBJECTIVE AND PRINCIPLE

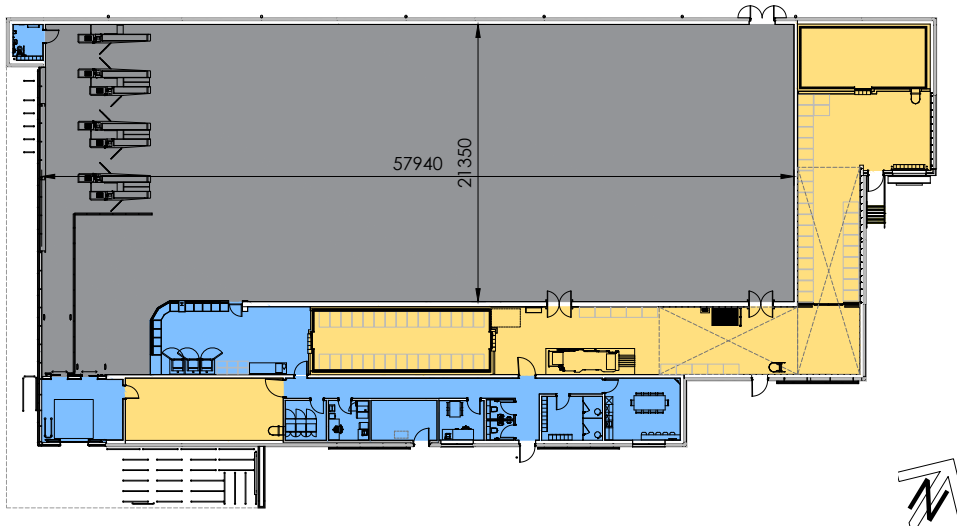
Lidl's proposed development will create a new neighbourhood foodstore with 1,284m² sales area (2,016m² gross internal floor area). The development will also provide a free customer car park and recessed loading bay to facilitate the delivery of goods to the store.

The store will be built in accordance with Lidl's brand new specification providing a lighter, more spacious sales area, with full height glazing to the front elevation, and with no suspended ceiling. Proposed elevations have been considerably designed along the main aspects, providing activity and identity to the public frontages which:

1. Respond to the geometry and topography of the site;
2. Maintain adequate clearances and safe distances to existing site boundaries;
3. Respect the urban edge and remain in keeping with other developments in the area;
4. Provide easy and safe vehicular and pedestrian access to the existing highway;

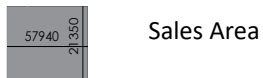
The proposals for the site layout and store will incorporate Lidl's corporate branding.





Proposed Ground Floor Plan

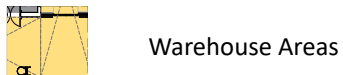
KEY:



Sales Area



Ancillary Areas



Warehouse Areas

PROPOSED LAYOUT

The proposed store has been positioned to the north east of the site. The eastern and southern areas of the site are to be customer parking with vehicle and pedestrian access.

The car parking spaces have been positioned to the principle elevations of the store. The car park area has been designed to allow easy manoeuvrability of vehicles and also safe and easy access to and from the proposed store.

Disability and parent and child spaces are provided adjacent to the store entrance to provide easy access for these users. Electric vehicle charge points are also provided adjacent to the car park entrance.

Goods deliveries will be made via the recessed loading bay which is situated adjacent to the warehouse space. The store's dedicated delivery facility will be accessed via an HGV grade vehicle ramp directly in front of the delivery pod door.

A vehicle tracking exercise has been carried out which confirms a delivery vehicle can access the delivery area during trading hours without having any adverse effect to the car parking arrangements.

While designing the proposed development the following key factors were taken into consideration:

- Market demand
- Surrounding properties
- Site constraints
- Vehicular and pedestrian circulation within the site
- Design guidance





Example images of a typical Lidl store

APPEARANCE

The site aesthetic has been considered as part of the proposal with the planned use of a limited but considered material palette affording the design coherency.

The West elevation facing Bulwark Road will implement 4m glazing to provide natural light and to help modernize the visual appearance of the store. The shop front will wrap around a fraction of the south side elevation to comprise the customer entrance lobby. With the 4m glazing wrapping around the corner of the store, the entrance lobby will have a welcoming open atrium ambience for customers to the Lidl store.

The elevations will consist of white cladding panels, with grey rendered plinth beneath. This palette will be complemented by silver eaves guttering and rainwater pipes.

The loading dock gable south elevation at the front of the store will complement the other elevations through using the same material composites and colour palette. A rendered treatment with the silver metal composite panels above comprise this elevation. The silver metal composite panels will increase in depth along this elevation due to the rake of the mono pitch roof.

Each elevation treatment responds to its specific context, whilst also addressing the development as a whole by:

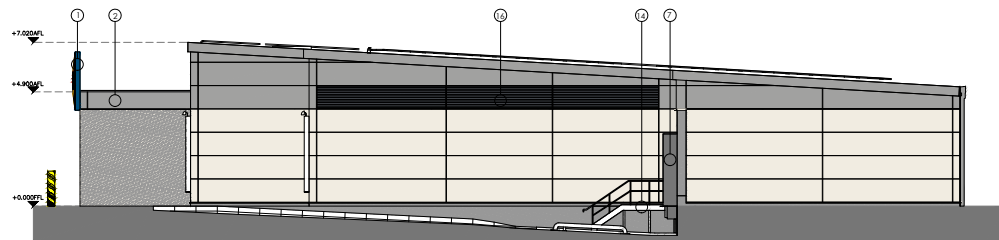
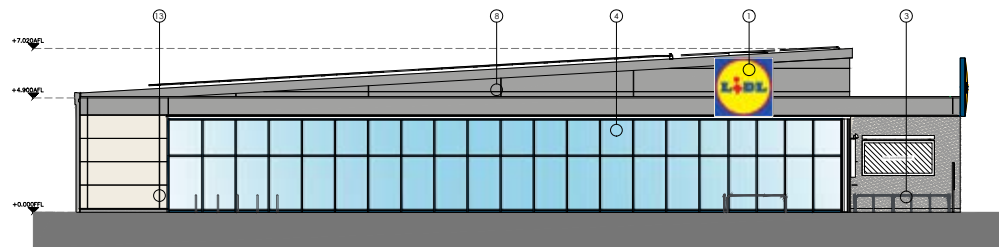
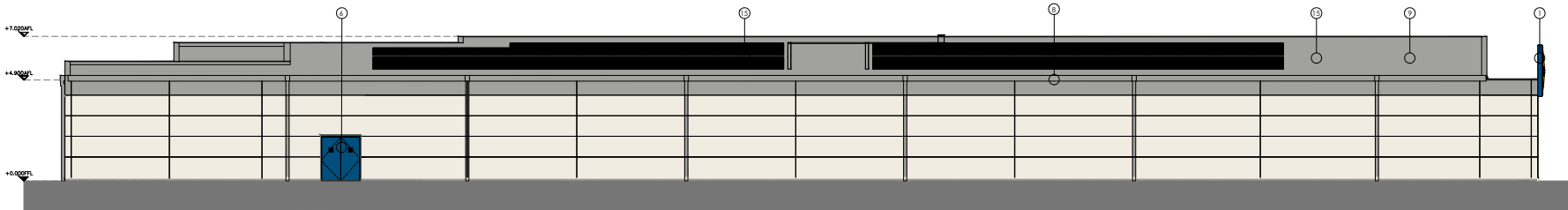
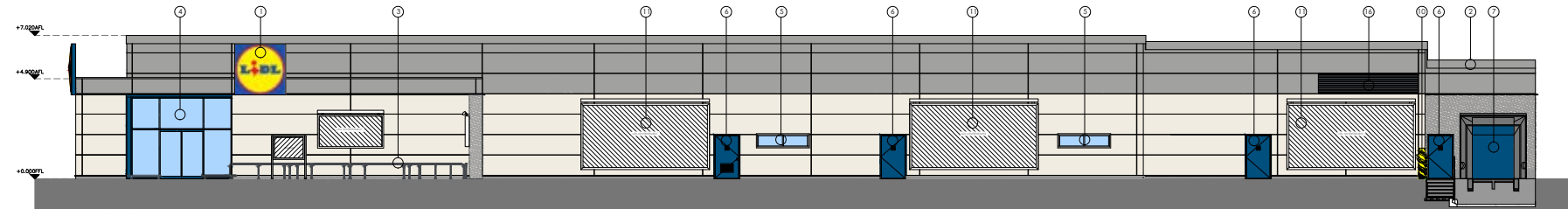
- 1) Using carefully selected robust and high quality architectural components
- 2) Using a well-chosen selection of robust and quality architectural components.
- 3) Using an appropriate architectural form
- 4) Referencing the scale of the building within the elevation treatments

While final confirmation of the exact material specification requires approval by the Local Authority, it is intended that the pallet of materials will consist of the following:

- Roof cladding – standing seam - RAL 9006 in silver
- Rainwater goods – aluminium - RAL 9006 down pipes silver
- High level eaves cladding, fascia and soffits colour silver - RAL 9006
- Upper external wall cladding - RAL 9010 white
- Lower external wall - plinth - RAL 9006 Render grey
- Doors/Windows/Curtain Wall - RAL 5010 colour, blue

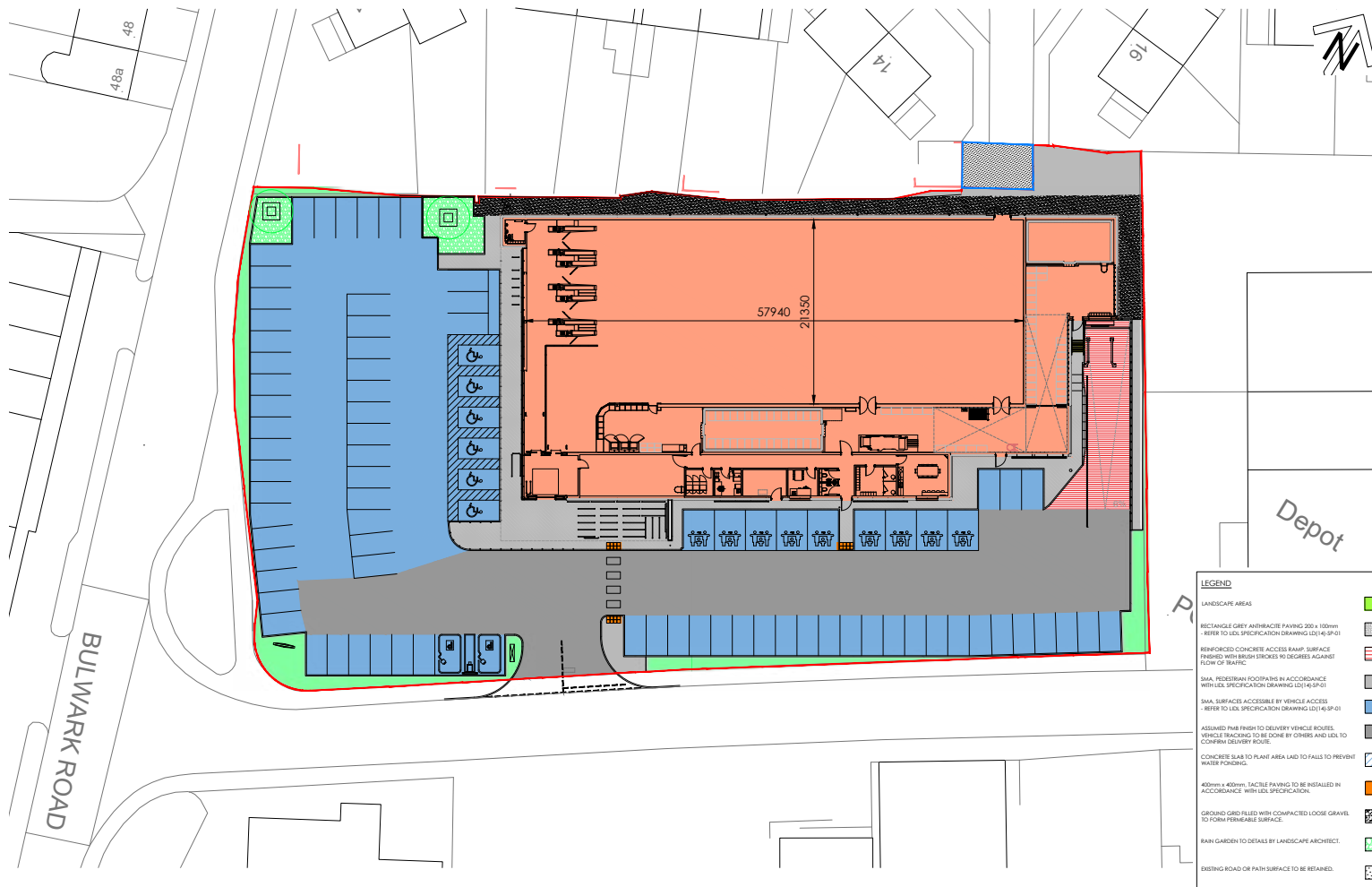


PROPOSED DEVELOPMENT - BULWARK ROAD, CHEPSTOW ELEVATIONS



- PROPOSED MATERIAL FINISHES**
DRAWINGS TO BE READ IN CONJUNCTION WITH L&L SPECIFICATION.
MATERIALS INDICATED ARE ILLUSTRATIVE ONLY AND MAY NOT MATCH THE FINAL FINISH.
- ① NEW LED SIGNAGE INTERNALLY ILLUMINATED 2500mm x 2500mm
 - ② PARKADE CLADDING COLOUR SILVER RAL 9004
 - ③ STAINLESS STEEL TOLLIE SATURALS
 - ④ PROPOSED FULL HEIGHT CURTAIN WALL GLAZING COLOUR BLUE RAL 5010
 - ⑤ PROPOSED WELFARE WINDOW COLOUR BLUE RAL 5010
 - ⑥ PROPOSED DOOR/USE DOORS/ UTILITY ROOM COLOUR BLUE RAL 5010
 - ⑦ PROPOSED DELIVERY DOCK SECTION DOOR COLOUR BLUE RAL 5010 WITH DOCK BRESTER
 - ⑧ NEW METAL CLADDING TO SOFFIT, FACIAS, NEW WING, FLASHING, GUTTERS AND RWP COLOUR SILVER RAL 9004
 - ⑨ PROPOSED CHAMBER ROOF CLADDING TO BE INSTALLED TO MATCH WALL CLADDING COLOUR GREY RAL 9004
 - ⑩ HGV BOLLARD 1500mm HIGH BLACK AND YELLOW
 - ⑪ WALL MOUNTED BILLBOARDS (DOWN INDICATIVELY SUBJECT TO SIGNAGE APPROVAL)
 - ⑫ STAINLESS STEEL BOLLARDS TO BE DOMED TOP MAMBE GRADE 316, 1000mm HIGH
 - ⑬ GALVANISED SHEFFIELD CYCLE STANDS
 - ⑭ GALVANISED STAINLESS STEEL DRIVERS STAIRS
 - ⑮ ROOF MOUNTED SOLAR PV PANELS
 - ⑯ VENTILATION LOUVERES COLOUR RAL 7034
 - ⑰ CLADDING PANELS IN WHITE RAL 9010
 - ⑱ CLADDING PANELS TO MATCH RAL 9004
 - ⑳ REDUCED FLUSH TO MATCH RAL 9004





BOUNDARY PROPOSALS

The perimeter landscaping and the proposed landscaped areas within the scheme will provide a framework for the site. For detailed landscape proposals refer to the Landscape Architect's drawings. Also provided within the planning application are Boundary Treatments and Site Finishes Plans.

SITE FINISHES

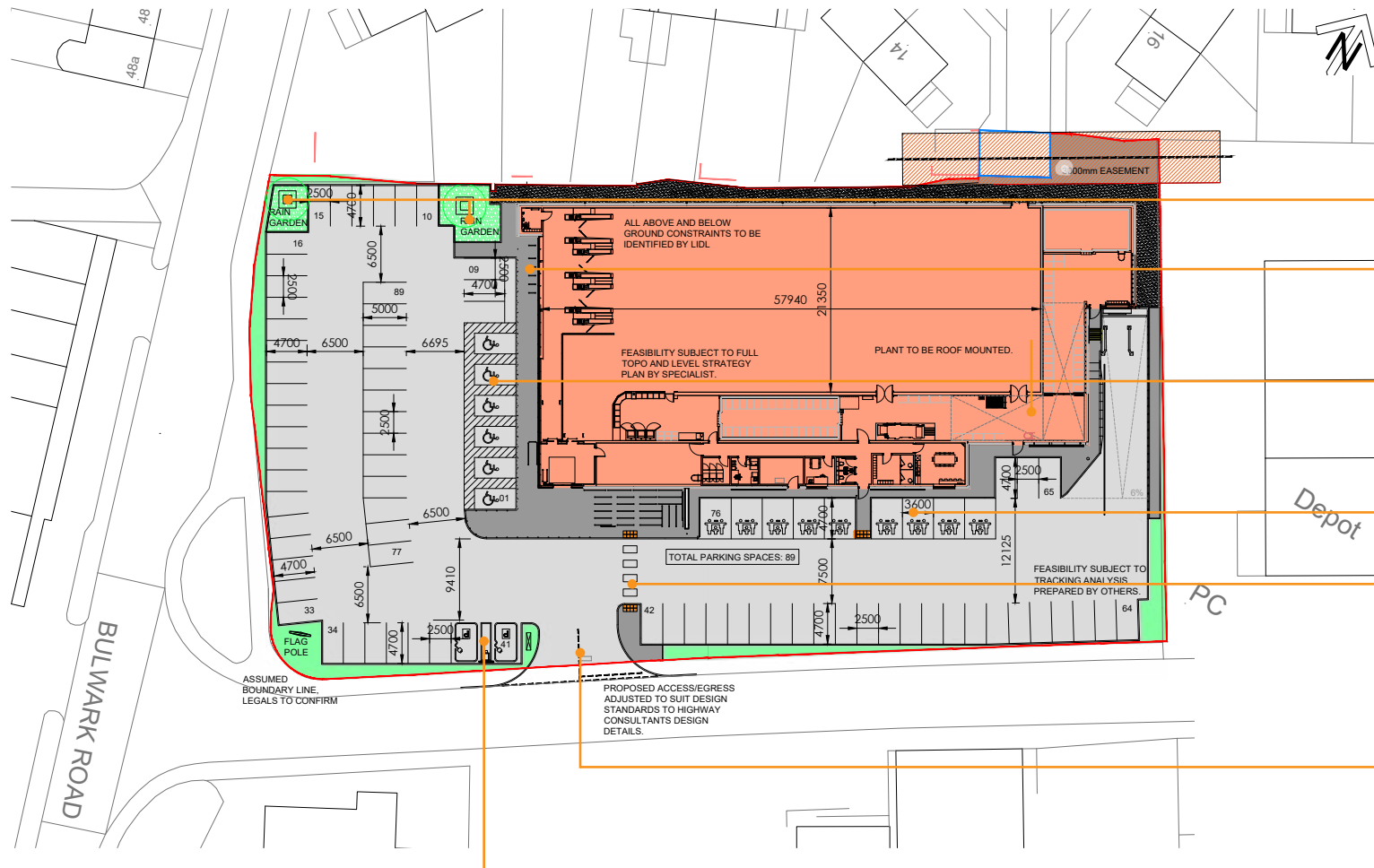
The proposed development has undergone a considered and detailed landscaping design in order to assist the development in assimilating into the surrounding area and to enhance the appearance and visual amenity of the site. The proposed retail development will incorporate landscaping to the northern, eastern, western and south boundary of the store.

Please refer to the site finishes drawing 20054 AD 115 – Proposed Site Finishes for further details.



PROPOSED DEVELOPMENT - BULWARK ROAD, CHEPSTOW

PROPOSED SITE PLAN



PROPOSED RAIN GARDENS

PROPOSED CYCLE PARKING

PROPOSED DISABLED PARKING SPACE

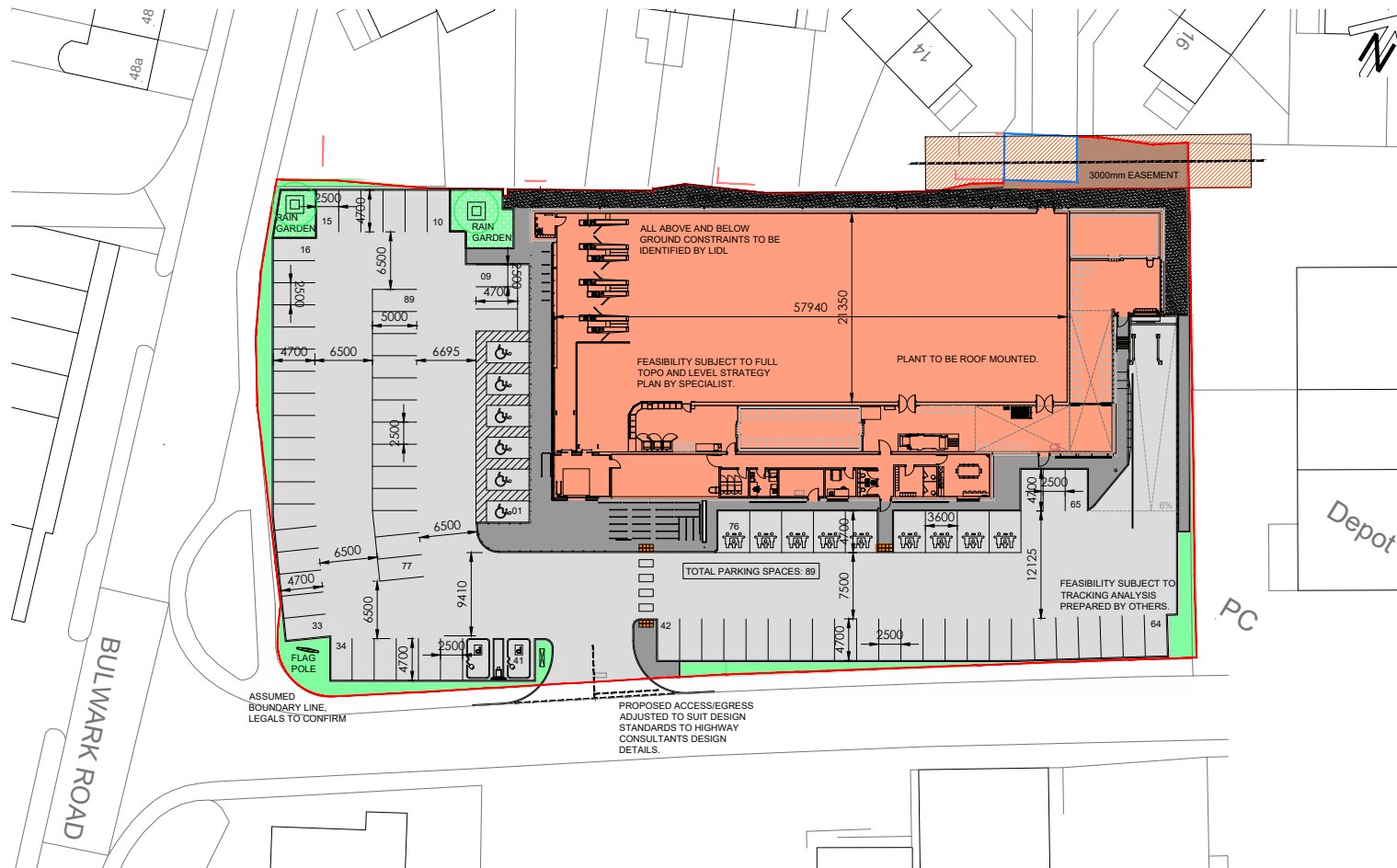
PROPOSED PARENT & CHILD SPACES

PROPOSED PEDESTRIAN ACCESS

PROPOSED ACCESS/EGRESS ADJUSTED TO SUIT DESIGN STANDARDS TO HIGHWAY CONSULTANTS DESIGN DETAILS.

PROPOSED ELECTRIC VEHICLE CHARGING SPACE





VEHICULAR ACCESS PROVISION

Existing vehicle access from Bulwark Road will ensure the provision of safe ingress and egress into the site. Adequate parking of 89 spaces has been provided and include Lidl spec standard, disabled, parent and child and electric vehicle charging car parking spaces.

Vehicle parking spaces provided are generously sized for improved customer access. Cycle storage is provided to the front of the store:

- Standard spaces - 4.7-5.0m x 2.5m
- Parent and Child - 4.7m x 3.6m
- Disabled - 6.0m x 3.6m (including 1.2m hatching zone)
- Electric Vehicle Charging Spaces - 6.0m x 3.6m

PEDESTRIAN ACCESS PROVISION

Pedestrian routes are provided through the site and cycle storage areas are identified on the layout. The proposed development will ensure a maximum 2% gradient across the site to allow for those with mobility impairments having no difficulties in accessing the store. A maximum of 6% gradient will be provided for the delivery ramp. Level access thresholds are to be provided into all the main entrance points into the food store unit. The scheme will be fully DDA compliant, including WCs and disabled parking spaces.





Images of an internal Lidl store

LIDL ENVIRONMENTAL POLICY

The scheme will be designed to comply with the latest government guidance and the new Part L Building Regulations. In addition, various sustainable measures will be implemented. These include but are not limited to:

- Sustainable surface water drainage scheme.
- Solar panels installed on the roof.
- Electric Vehicle Charging spaces.
- Recycled materials in insulation and floor finishes.
- Use of water-based paints on steelwork.
- High efficiency LED lighting incorporated internally and externally.
- Lighting to be installed with movement detectors.
- Dual flush toilets.
- Sensor taps to reduce water consumption.
- Improved air leakage and enhanced U-Values where possible.
- Building management system controlled lighting by Lux sensors.
- Environmental waste removal and management policies.
- Delivery noise reduction through quiet, manually operated bridging plates and dock shelter, used in conjunction with sloped loading ramp.

As a discount food retailer, Lidl's supply chain and long-term business success depends on the health of the natural environment. Lidl therefore takes its responsibility as a member of the community very seriously in ensuring it reduces its carbon footprint wherever possible.

Environmentally sustainable design solutions have been incorporated into various stages of the proposed development, affecting the internal design, external appearance and the foodstore's relationship with its context. Energy demand is reduced in the first instance through passive design and a consideration of efficient operational procedures ensures that the proposed foodstore's ongoing environmental impact is minimised.



PEDESTRIAN MOVEMENT

Pedestrian access to the site is provided at points along Bulwark Road and Bulwark Industrial Estate access road to the south east and south of the site respectively. These access points connect directly into the extensive pedestrian network running through Bulwark and the wider Chepstow area respectively.

Footways of approximately 2m in width are present along both sides of both Bulwark Road and Bulwark Industrial Estate access road. Directly to the south east corner of the site, a zebra crossing is located across Bulwark Road, and a dropped kerb crossing point is provided across Bulwark Industrial Estate access road. A signalised pedestrian crossing point is also located across Bulwark Road approximately 75m south of the site.

Pedestrian access to the site will be provided at a new vehicular access point along the Bulwark Industrial Estates access road with internal footways connecting directly into the existing footway along the northern edge of the estate access road.

Cycling Access

Cycling in the immediate vicinity of the site is primarily accommodated on road. Connections are available however into the National Cycle Network (NCN). These NCN routes offer a mix of on-road and off-road routes to neighbouring larger towns/cities

such as Newport and Bristol.

With its close proximity to several national cycle routes, as well as Chepstow railway station and the number of residential settlements within the 4km and 12km cycle catchment areas, the site is well located to attract staff and customer cycle trips.

A total of 6 Sheffield cycle stands are included within the site proposals, which will allow parking for up to 12 bicycles, located in a prominent and convenient location to promote natural surveillance; the cycle stands are also covered. This generous provision will help encourage cycle modes of travel.

Public Transport

The nearest bus stop to the site is the Social Club stop located on Bulwark Road, directly to the south west corner of the site (less than a 20m walk from the site boundary). This stop operates via a bus bay, with a sheltered waiting area for southbound services, and with a flagged and timetabled on-road stop for northbound services.

The Social Club stop provides access to the Newport Bus services T7 and C1. Service T7 is a long-distance service routing between Magor and Bristol (via Chepstow), whereas service C1 is a local service routing within Chepstow. Regular bus services are available connecting the site to other

residential areas within Chepstow, and to neighbouring towns and cities such as Caldicot, Magor, and Bristol.

Chepstow bus station is also located approximately a 1.2km walk north from the site and offers access to a number of additional local and regional bus services. Although located outside the recommended 400m walking distance to a bus stop, the bus station does offer a viable option, especially for commuting trips to the proposed site.

The site is therefore considered to be in a favourable location to offer staff and visitors to the site opportunities to travel via bus. The nearest train station to the site is Chepstow, approximately 1.6km north of the site, an approximate 5-minute cycle. Chepstow station is operated by Transport for Wales (TfW) and provides access to regular rail services (at least 1 service per hour in each direction) routing between west and east Wales stations. Stations within an approximate 30-minute rail travel time catchment to Chepstow include Caldicot (8-minutes) and Newport (25-minutes) to the west, and Lydney (8-minutes) and Gloucester (30-minutes) to the east.

Travel by train as part of a longer multi-modal trip including cycling is therefore an option to users of the site, especially as part of a commuting trip for staff members.



Vehicular Access

The site is accessed off the Bulwark Industrial Estate access road. This road is approximately 250m in length, running between Bulwark Road at its western point, and terminating within the Industrial Estate at its eastern point.

The Bulwark Industrial Estate access road connects to Bulwark Road via a 3-arm priority junction (with Bulwark Road forming the major arms). Access to the site is taken by an informal dropped kerb arrangement, at a point approximately 60m east of this junction.

The Bulwark Industrial Estate access road is approximately 7m in width, with footways provided along both sides of the carriageway. The road is subject to a 30mph speed limit, and is street lit within the vicinity of the site.

Vehicular access to the site is proposed via a new formal priority junction located along Bulwark Industrial Estates access road. This junction will be located at a point approximately 50m east of the junction with Bulwark Road and will be designed to comply with local and national design guidance.

Conclusion

The design of the proposed development has been mindful of the access policies in the LDP including Policies S16, MV2 and DES1.

The site is well located to encourage sustainable modes of travel for staff and customers living in the surrounding residential areas of Chepstow. The cycle and bus networks through the local area will also allow opportunities for sustainable travel for customers and staff.



COMMUNITY SAFETY

The proposed site layout has adopted the following 'core principles' of designing for community safety and which are intended to improve the design and management of the physical environment and to promote community cohesion.

1. Integrated Approach:

The proposed scheme has been designed to ensure a safe and accessible environment for customers arriving on foot, cycle and car, whilst ensuring that the site can be safely serviced by delivery vehicles.

2. Environmental Quality and Sense of Ownership:

The proposed re-development of the site will improve the environmental quality of the area, providing a significant improvement to the Bulwark Road street scene, with the removal of a dilapidated building with an attractive active frontage which promotes natural surveillance of the site and a sense of ownership.

3. Natural Surveillance

The position of the retail store on the northern boundary of the site ensures that the car park and cycle parking areas are in full view from Bulwark Road and the estate road. Internally, the glazed lobby area overlooks parts of the car park and cycle parking spaces.

4. Access and footpaths

The proposed development ensures that all pedestrian links through the site provide safe and dedicated pedestrian walkways and crossings.

5. Lighting

Lighting columns will be provided for car parking and pedestrian routes around the site to reflect their requirements.



REFERENCES

Supplementary documents that have been used to develop the scheme proposals include:-

- National Planning Policy Wales
- Monmouthshire LDP
- CDM2015 requirements and future maintenance

LIST OF DRAWINGS

- 20054 AD 100 – Location Plan
- 20054 AD 101 – Existing Site Plan
- 20054 AD 102 – Demolition Plan
- 20054 AD 110 – Proposed Site Plan
- 20054 AD 111 – Proposed Building Plan
- 20054 AD 112 – Proposed Roof Plan
- 20054 AD 113 – Proposed Elevations
- 20054 AD 114 – Proposed Boundary Treatments
- 20054 AD 115 – Proposed Site Finishes
- 20054 AD 118 – Proposed Topo Overlay Plan
- 20054 AD 123 – Proposed Site Section
- 20054 AD 126 – Proposed Street Scene
- 20054 AD 131 – Proposed EV Infrastructure Plan





Unit 5 Mercury Quays,

Ashley Lane, Shipley

West Yorkshire, BD17 7DB

Tel +44 (0) 1274 591 188

Email mail@onedesignarchs.com

Web www.onedesignarchs.co.uk

